



Devil's Bowl Speedway
1711 Lawson Road
Mesquite, TX 75181
Web: www.devilsbowl.com
E-mail: info@devilsbowl.com

(972) 222-2421
Fax: (972) 222-8901

Devil's Bowl Speedway

2008 Racing Season Late Model Rules

Miscellaneous

- A) Any person seated in a car that is moving under its own power, or being pushed, must wear seatbelts and a Snell 95 or newer full face helmet. This includes moving to fuel pump, to the scales, and the lineup.
- B) Any car unable to leave the pit under its own power, or must be push started will be sent to the rear of the field.
- C) ALL CARS IN THE TOP 5 AFTER THE FEATURE MUST GO DIRECTLY TO THE SCALES.
- D) No mirrors, wings, two way radio or communication from pit to car, or car to pit allowed.
- E) ALL CARS MUST PACK THE TRACK, OR FORFEIT THE RIGHT TO HOTLAP. NO EXCEPTIONS.
- F) TRACK HAS FINAL SAY ON ALL RULES. IF A TRACK OFFICIAL, OR A REPRESENTATIVE OF THE LATE MODEL CLASS, REQUEST A CHANGE, OR ANY RULE CHANGE IS ANNOUNCED DURING THE NIGHT, OR DURING THE SEASON, YOU WILL BE EXPECTED TO MAKE CHANGES BY THE TIME INDICATED.
- G) All cars must be presented for inspection at any time if requested to do so by a track or late model official.
- H) TRACK AND LATE MODEL OFFICIAL RESERVE THE RIGHT TO CHANGE OR UPDATE RULES AT ANY TIME.
- I) All weight bolted to car must be painted white and have the car number painted or stamped on it.
- J) All drive shafts must be painted white with the cars number painted on it.
- K) Cars will be teched and heads will be pulled after the 3rd feature win, or at the tracks discretion. The tech will be performed by the track tech officials, and they have the final word in all tech matters.

Protest

(NOTE: IN THE INTEREST OF FAIRNESS ALL TECH WILL BE OVER SEEN BY A TRACK OFFICIAL. SHOULD THE OFFICIAL CHOSEN HAVE AN INTEREST IN THAT CAR, ANOTHER OFFICIAL WILL BE USED.

TRACK HAS FINAL WORD ON ALL RULES, PROTEST AND PROCEDURES.

- A) Protest fee and request must be made in cash to acting pit steward at the pit board, or to track appointed tech official.
- B) Any driver or car owner who wishes to protest a car in the same division may do so as long as said persons car is present and starts the heat race.
- C) All protest must be made before the green flag drops on the A-main event..

- D)** Protest fee is \$250.00 and must be in cash with \$50.00 of the fee going to the tech official. If the car being checked is found legal the protest fee goes to car owner/driver to help pay for gaskets etc... If it is found illegal the protest money will be returned minus the \$50.00 that went to the tech official.
- E)** If the car being protested is found illegal that car and driver loses all points for the season and all winnings, awards, etc. for the night.
- F)** The driver or car owner of the car being protested has the right to face his/her accuser and request the car making the protest to be teched as well, as long as he/she is willing to put up the protest fee before tech begins on his/her car. In other words, if the #00 protest the heads on the #88, the #88 has the right to have the heads on the #00 checked as well.
- G)** Only the registered car owner or registered driver may make a protest, no crew, wives, friends, kids, etc....

Entry Fee

- A)** \$30.00 entry fee must be paid before hot laps or you will not run your heat.
- B)** A representative of the late model division will collect the fee, BUT IT IS YOUR RESPONSIBILITY TO SEE THAT THE FEE IS PAID.
- C)** Any questions should be directed to Harry Hronas during weekday business hours at 214 324-0602.

Weight

- A)** MINIMUM WEIGHT OF ALL CARS AFTER ALL RACES (HEATS, DASHES AND MAINS) IF YOU RUN A SUPR SPEC MOTOR IS 2150 POUNDS ALL OTHER MOTORS MINIMUM WEIGHT IS 2350 POUNDS ON THE TRACK SCALES LOCATED AT THE BACK PIT LANE BEHIND THE CONCESSION STAND.
- B)** Again, all weight bolted to the car must be painted white with the car's number painted or stamped on it.
- C)** THE TRACK HAS THE RIGHT TO DO SURPRISE WEIGHT INSPECTIONS AT ANY TIME, THIS INCLUDES BEFORE OR AFTER ANY RACE OR HOTLAP SESSION.

Tires and Wheels

- A)** Wheels may be steel or aluminum up to 14 inches wide.
- B)** Bead locks are allowed on outside of all wheels.
- C)** Each wheel must be attached to the car with a minimum of five (5) one inch (1") lug nuts.
- D)** No knockoffs allowed.
- E)** No bleeders or pop off valves allowed.
- F)** Front tires may be Hoosier M45, RC-3, spec 55 or WRS 55 Hoosiers, or McCreary E-C2 only.
- G)** Rear tires may be Hoosier M-45, spec 55, WRS 55 or McCreary E-C2 only.
- H)** Grooving, siping and grinding of tires is allowed.
- I)** TIRE SOFTENING IS NOT ALLOWED.

Brakes

- A)** All cars must have working three (3) wheel brakes. Four (4) wheel recommended.
- B)** No carbon fiber allowed.

Chassis /Frame

- A)** No aluminum frames or aluminum clips allowed.
- B)** All frames must be of steel construction and must be a minimum of 2"x 2" square or

rectangular, or 1 3/4" outside diameter round tubing with a minimum thickness of .083 inches.

- C) No outside front bumpers allowed.
- D) Rear Bumper must be no wider than the centerline of the rear tires.
- E) Minimum wheel base is 103 inches.

Roll cage

- A) All cars must have a suitable steel roll cage protecting the drivers compartment.
- B) Side roll bars (door bars) are mandatory and must extend into the door panels.
- C) A minimum of 3 bars, at least 1 1/2 inches in diameter and a minimum of 0.095 inches thickness to be used on the left side.
- D) Roll cages must be welded to chassis.
- E) Driver must sit on left side of driveshaft loop.

Battery

- A) Must be securely mounted and be capable of starting.

Fuel Cell and Fuel

- A) One approved fuel cell contained in a steel box made of 20 gauge steel.
- B) A minimum of four (4) one-eighth (1/8) inch holes in bottom of box for drainage.
- C) A one-way rollover or check valve is mandatory in vent lines.
- D) Methanol may be used, no additives, except top oil, allowed.
- E) Gasoline may be used, on oxygenates, nitro compounds or other nitro containing compound allowed.
- F) No fuel injection allowed.

Motor Setback

- A) Motor setback is 6 inches from center of top ball joint to center of #1 sparkplug. This is the maximum setback allowed.

Mufflers and Headers Mandatory

- A) Collector type headers required.
- B) Custom craft 3 ring muffler insert mandatory. 3 1/2 inch diameter maximum. Long end must go towards the motor. Insert must be same size as collector. No 3 inch inserts in 3 1/2 inch tubes.
- C) All cars must have turndowns.
- D) Muffler Rule change was made by Lanny Edwards, Track Owner, and will be enforced by the track tech officials.

Rear ends

- A) Any passenger car or truck rear end, may be locked.
- B) Quick change rear ends allowed.
- C) No independent rear ends allowed.

Transmission and Clutch

- A) Must be mounted to bellhousing on back of motor.
- B) Must have working forward and reverse gears.
- C) Car must be able to leave pit under it's own power.

Carburetor

- A) One four (4) barrel carburetor or one two (2) barrel carburetor allowed.
- B) A maximum of one (1) spacer between carburetor and intake, no thicker than one (1) inch.

Heads

Aluminum heads may only be Brodix spec or SUPR spec heads and must be stock. No alterations, no port matching, no polishing, no milling or any other work allowed. All SUPR spec motors must use SUPR spec rules.

Steel Head Motor Rules

- A) Any V8 engine produced by an American auto manufacturer not to exceed one (1) intake and one (1) exhaust valve per cylinder.
- B) Cast-iron block only.
- C) Aluminum intake allowed. Intakes may be ported on all steel head motors.
- D) Maximum displacement to include wear:
 - 1) Chevrolet 365 cubic inches
 - 2) Chrysler 375 cubic inches
 - 3) Ford 365 cubic inches
 - 4) Others 365 cubic inches

Note—A new cast head is being offered by Chevrolet, but is still unavailable for inspection. This head part # 25534371 is not legal at this time (until availability and inspection).

- E) 23 degree steel bowtie or steel OEM heads, port and polishing is allowed. No dart, pro brezinski or other aftermarket heads allowed.
- F) Angle milling is allowed.
- G) 23 degree head castings only.
- H) Flat top pistons only.
- I) Steel rods, steel crank only.
- J) Type of cam optional, no overhead cams.
- K) Roller cams and roller rockers are allowed.
- L) Battery type ignition, or magneto allowed. Must be in stock location.
- M) No crank triggers allowed.
- N) Dry sump oil systems OK.
- O) No traction control devices of any kind allowed.
- P) No injection of fuel, water, nitrous oxide allowed.
- Q) No turbo charging, supercharging, or inter cooling allowed.
- R) Any bore and stroke combo as long as it doesn't exceed C.I. Limit.
- S) Aftermarket blocks, cranks, rods, pistons, etc... allowed.

***Note*—We use the S.U.P.R. Spec motor rules as they appeared on the 2005 S.U.P.R. Website, and Rulebook. However, we do not claim to run S.U.P.R. rules or plan to run their rule package in our weekly shows. Any person or persons wishing to run under full S.U.P.R. rules should attend a S.U.P.R. sanctioned event, or run at a track which has adopted their rule package as it's own. We say this to clear up any confusion that may have been caused by past rule changes or track press releases.**

Spec Engine

- A) Minimum 350 cubic inches (ci). Maximum 362 ci. Using steel block.
- B) Maximum bore size is 4.060 flat top pistons only.
- C) Bore stroke combinations to achieve this 3.480 or 3.500 on Chevy and 3.500 on Ford. Steel block for Chevy will have 9.0 inch deck.
- D) Brodix SUPR spec heads (with 60cc minimum combustion chambers).
- E) Chevy motors must have a Brodix HV1 intake with no modifications except for port matching 3/4 to Felpro 1206.
- F) Ford motors must have an Edelbrock Victor Jr. intake with no modifications except for port matching 3/4 to Felpro 1262.
- G) **NO LIGHTENING OF BLOCKS.** If found, a 25lb. Weight penalty will be assessed in front of the motor plate.

Addition to SUPR spec paragraph

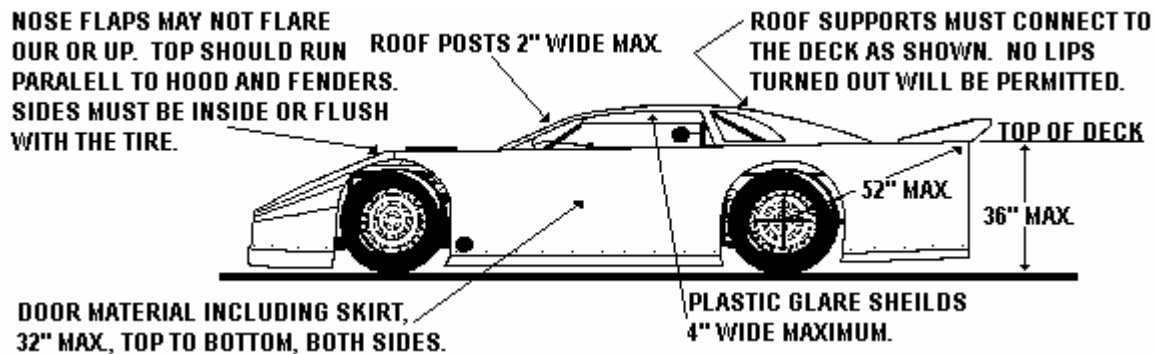
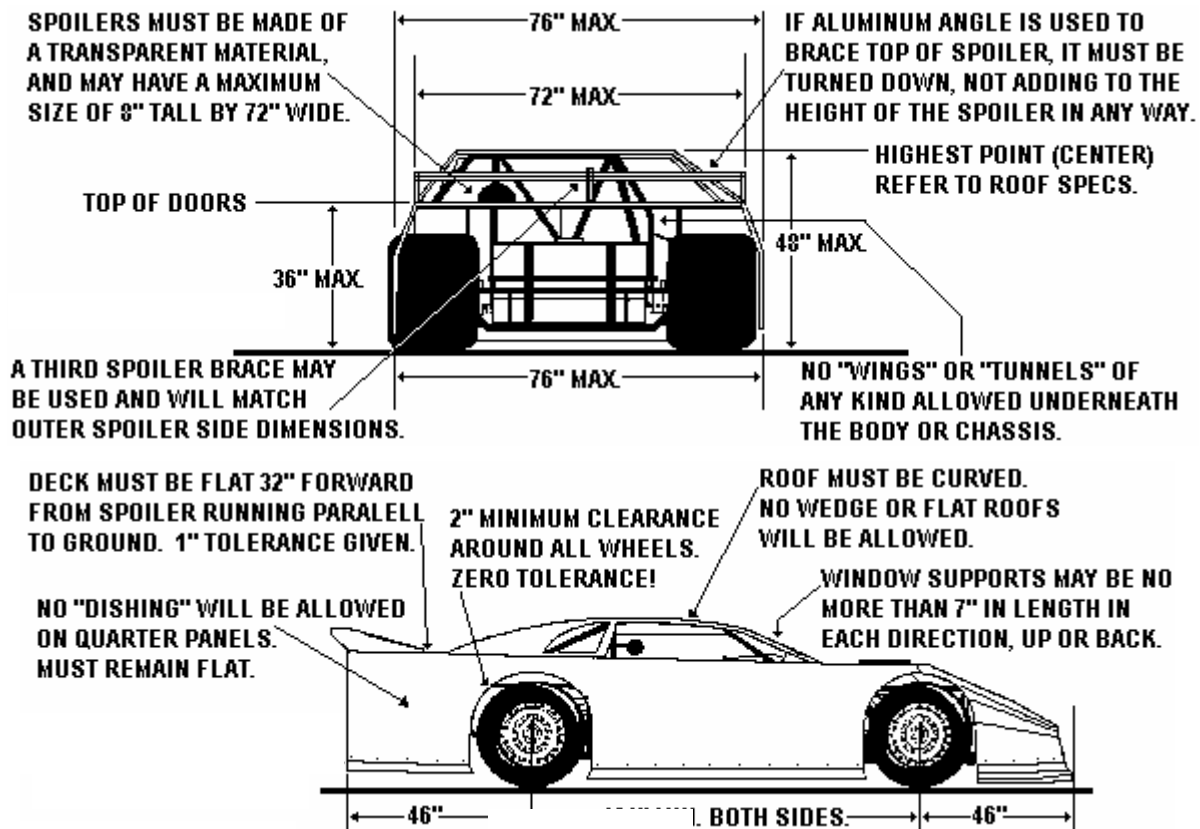
- A) SUPR spec heads with SUPR spec logos.
- B) Chevy SUPR spec head with SUPR cast logo.
- C) Ford SUPR spec head with SUPR logo on the outside and spec logo on intake and exhaust ports.
- D) Intake port opening no larger than original opening and may be port matched, Chevy to Felpro 1206, Ford to Felpro 1262. Chevy intake opening may be ground or polished 3/4 inch or no farther into the port than the closest edge of the closest letter of the SUPR logo. Ford intake may be polished or ground 3/8 inch. Intake port polishing will be allowed no more than 1 1/2 inch below the bottom of the original seat ring on the backside of the bowl area, and no more than 1 inch of the short side. Any opening up of the head past these points will be illegal. Polishing of the exhaust ports is permitted as long as the logo and port shape is not altered. No intake or exhaust port relocation, raising, enlargement or reshaping of any kind allowed. Valve angle and or placement may not be altered in any way on the SUPR spec head.
- E) In the rules above, if it doesn't say you can, then don't.

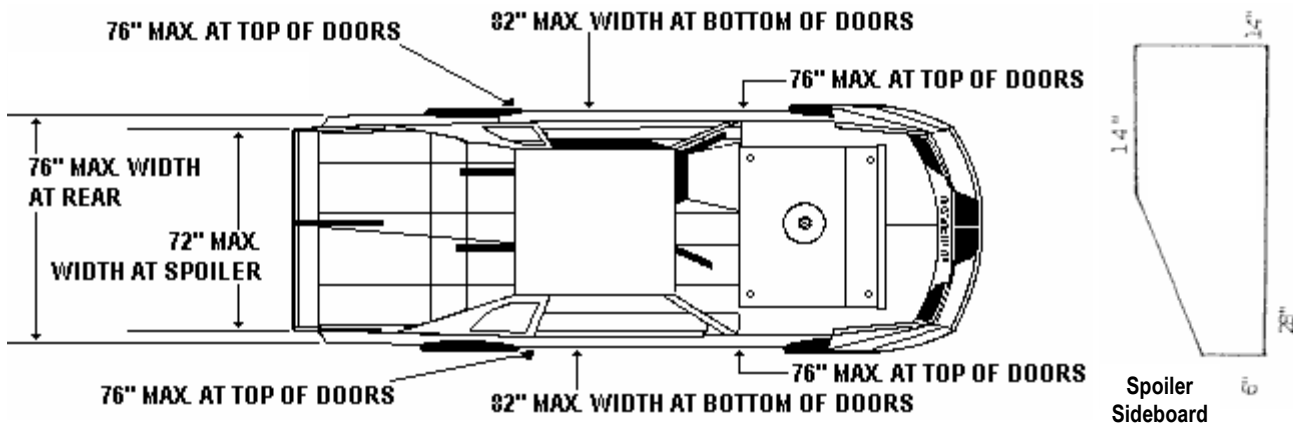
Note—These head and intake manifold combinations were designed with one thing in mind, to have equal air flow. If anything is done to increase airflow above original specs by manufacturer, they will be illegal. S.U.P.R. logo must remain, with NO changes, in all locations. Flat top pistons ONLY. No titanium valves, crankshafts or connecting rods.

Body Rules

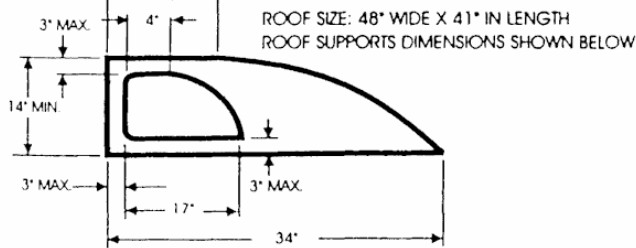
- A) Standard dirt style bodies are required. A stock appearing nose piece or sloped nose is legal. No wedge style bodies, no roof mounted spoilers or wings of any kind..
- B) Roof must be stock appearing and level (must run parrell to body).
- C) Spoiler must be transparent and have a maximum height of 12 inches including all lips, bends and the hinges.
- D) Spoiler sideboards (see body diagrams).

Graphics provided by SUPR Late Model Series Racing — "S" SUPR Spec car

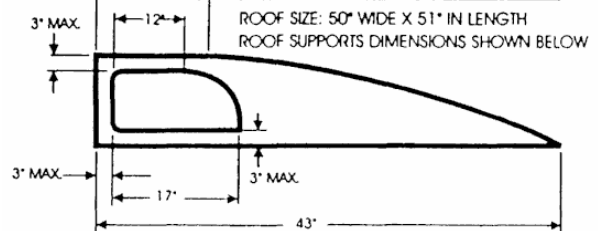




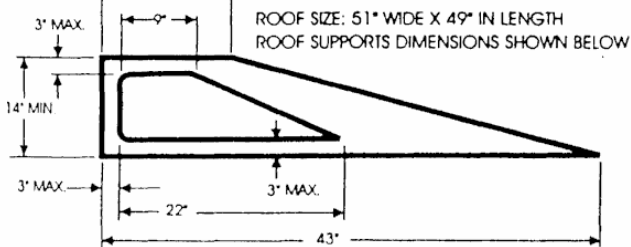
2002 Ford Mustang



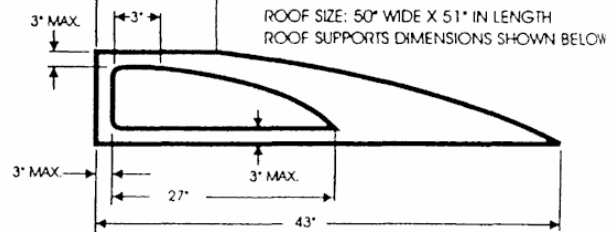
2002 Monte Carlo



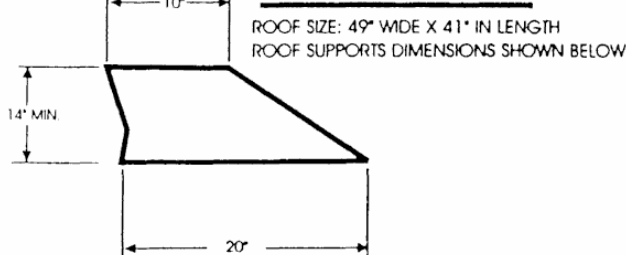
2002 Dodge Avenger



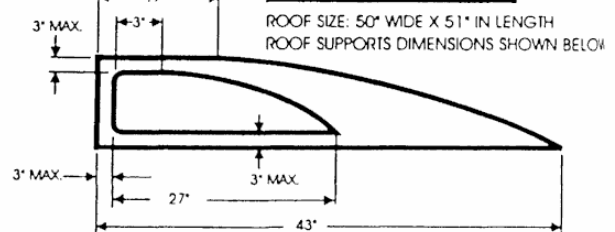
2002 Grand Prix



2002 Camaro



2002 Taurus



E) See diagrams for body dimensions.

Front Suspension

- A)* Aftermarket upper and lower A arms, rack and pinion steering and or coil overs allowed.
- B)* Upper buckets allowed.
- C)* Aluminum hubs and aftermarket spindles allowed. Pitman arm, center link, idler arm and tie rods may be aftermarket.
- D)* Weight jacks are allowed on all corners of cars.

Rear Suspension

- A)* Any rear suspension, other than independent rear suspension is allowed on all cars.
- B)* Quick-change rear-end allowed.
- C)* Aluminum hubs allowed.

Bumpers

- A)* Formed front bumpers mounted under the nosepiece that conform to the nose..
- B)* A pickup loop 12 inches wide, and eight (8) inches tall on stock nose cars may extend out of nosepiece.
- C)* Pickup loop on wedge nose cars may be no wider than front frame horns to lineup with rear bumper or fuel cell protection loops.
- D)* Rear bumper must have no sharp edges, and must stop flush or roll into quarter panel.
- E)* No nerf bars, kick rails or rub bars on outside of doors.
- F)* All cars must have fuel cell protection loop.

SUPR Type Chassis

- A)* Any pro late model chassis such as Shaw, GRT, Rocket, etc..
- B)* No aluminum frames. All frames must be steel. Construction must be a minimum of 2x2 inch square or 1 3/4 inch round with a minimum thickness of .083..
- C)* No front bumper allowed. Rear bumper allowed with no extensions except push bar.
- D)* Push bar must not extend more than six (6) inches beyond quarter panels.
- E)* Rear bumper can be no wider than center of rear tires.
- F)* All cars must have suitable roll cage protecting the drivers compartment.
- G)* Side roll bars are mandatory and must extend into the door panels.
- H)* A minimum of three (3) bars, at least 1 1/2 inches in diameter, and a minimum of 0.095 inch thickness to be used on left side.
- I)* Roll cages must be welded to frame.
- J)* Driver must sit on left side of driveshaft loop.
- K)* Minimum wheelbase is 103 inches.

**IF IT DOESN'T SAY YOU CAN DO IT,
YOU CANNOT!!!!**

**For more information contact Devil's Bowl Speedway
972-222-2421 (e-mail: info@devilsbowl.com)**